



**City Of Kingston
Municipal Accessibility
Advisory Committee
Meeting Number 03-2019
Addendum
Thursday, April 4, 2019 at 1:00 p.m.
Press Lounge, Portsmouth Olympic Harbour**

6. Briefings

- b) Anthony Simmons, Manager, Construction, will be present to speak to the Committee regarding Update on 2019 Construction Projects and Standards for Engineering Projects.

7. Business

- b) **Update on 2019 Construction Projects and Standards for Engineering Projects**

The Report of the President & CEO, Utilities Kingston (MAAC-19-007) is attached.

Schedule Pages 1 – 11

Recommendation:

This report is for information purposes only.



City of Kingston
Information Report to the Municipal Accessibility Advisory Committee
Report Number MAAC-19-007

To: Chair and Members of the Municipal Accessibility Advisory Committee

From: Jim Keech, President & CEO, Utilities Kingston

Resource Staff: Mark Campbell, Director of Engineering Services

Date of Meeting: April 4, 2019

Subject: Update on 2019 Construction Projects and Standards for Engineering Projects

Executive Summary:

This report is intended to provide the Municipal Accessibility Advisory Committee (MAAC) with the Construction Standards that the Engineering Services Department will be utilizing for all of the 2019 Capital Right-of-Way Projects. For these projects our goal is to comply with both the Integrated Accessibility Standards Regulation under the Accessibility for Ontarians with Disabilities Act, 2005 and City standards.

An overview of the 2019 Engineering Projects are included in this report which will be discussed at the April 4, 2019 MAAC meeting.

Similar to previous years, Engineering staff are requesting that MAAC assign a project team that could assist if and when required with the review of specific projects as the need arises.

Recommendation:

This report is provided for information purposes only.

April 4, 2019

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Authorizing Signatures:

ORIGINAL SIGNED BY PRESIDENT & CEO, UTILITIES KINGSTON

**Jim Keech, President & CEO,
Utilities Kingston**

ORIGINAL SIGNED BY CHIEF ADMINISTRATIVE OFFICER

**Lanie Hurdle, Acting Chief Administrative
Officer**

Consultation with the following Members of the Corporate Management Team:

| | |
|---|--------------|
| Gary Dyke, Commissioner, Corporate & Emergency Services | Not required |
| Peter Huigenbos, Acting Commissioner, Community Services | Not required |
| Desirée Kennedy, Chief Financial Officer & City Treasurer | Not required |

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Options/Discussion:

The Engineering Services Department is responsible for the Capital construction/reconstruction of linear features (for example, sidewalks, roads, etc.) within the City’s Right-of-Way (ROW). As part of these services, the department regularly reviews methods in which user accessibility can be enhanced, particularly when a site or asset is being built, or reconstructed. Existing City standards such as para-ramps, driveway depressions, sidewalk joint details, bus stop pads, rest area pads, etc., attached as Exhibit A, coupled with the Integrated Accessibility Standards Regulation, provide direction on the appropriate methods to address accessibility.

These standards, guidelines, and best practices provide the Engineering Department with sufficient guidance for the majority of the changes made to the roadway assets. These existing standards have been reviewed and approved based on input from the Municipal Accessibility Advisory Committee (MAAC) in previous years.

An overview of the 2019 Engineering projects are referenced below and will be discussed at the April 4, 2019 MAAC meeting, with the intention of seeking input from the MAAC on the various standards for consideration during the design and construction phases.

2019 Projects include:

| Location | From (Street) | To (Street) |
|--|-----------------------------------|--------------------|
| Collingwood Street | Earl Street | Couper Street |
| John Counter Boulevard (including bridge) | Indian Road | Princess Street |
| Sidewalk Program | Various Locations | Various Locations |
| Gardiners Road (intersection) | Centennial Drive / Creekford Road | |
| Resource Road and Venture Drive Extensions | Centennial Drive | Centennial Drive |
| Bayridge Drive | Princess Street | Cedarwood Drive |

Engineering works closely with other City departments including Parking Services, Transportation Services, and Kingston Transit to manage parking, traffic and transit issues during the design and construction, to ensure efforts are coordinated.

Engineering staff are requesting that MAAC assign a project team that could assist if and when required.

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Existing Policy/By-Law:

Not applicable

Notice Provisions:

Not applicable

Accessibility Considerations:

An overview will be presented at the April 4, 2019 MAAC meeting which will outline a number of projects that are underway within the Engineering Services Department that seek to improve the accessibility of the City's roads and sidewalks.

Financial Considerations:

All projects discussed in this report are being funded through the existing capital budget.

Contacts:

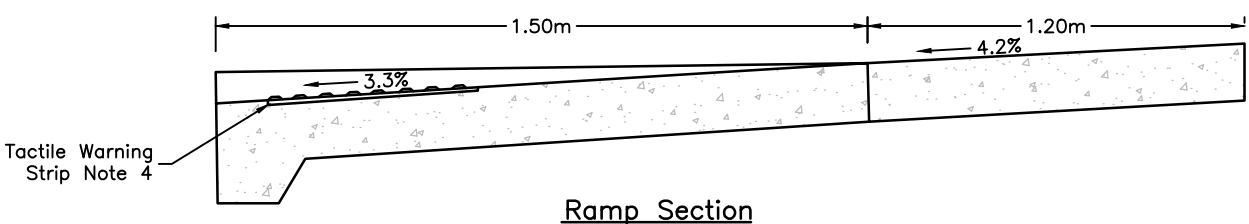
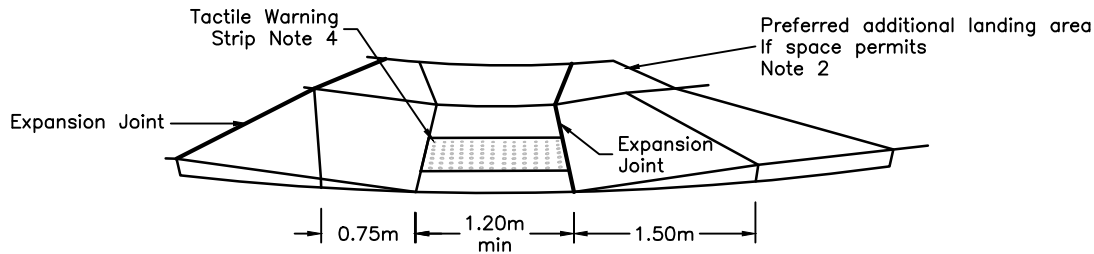
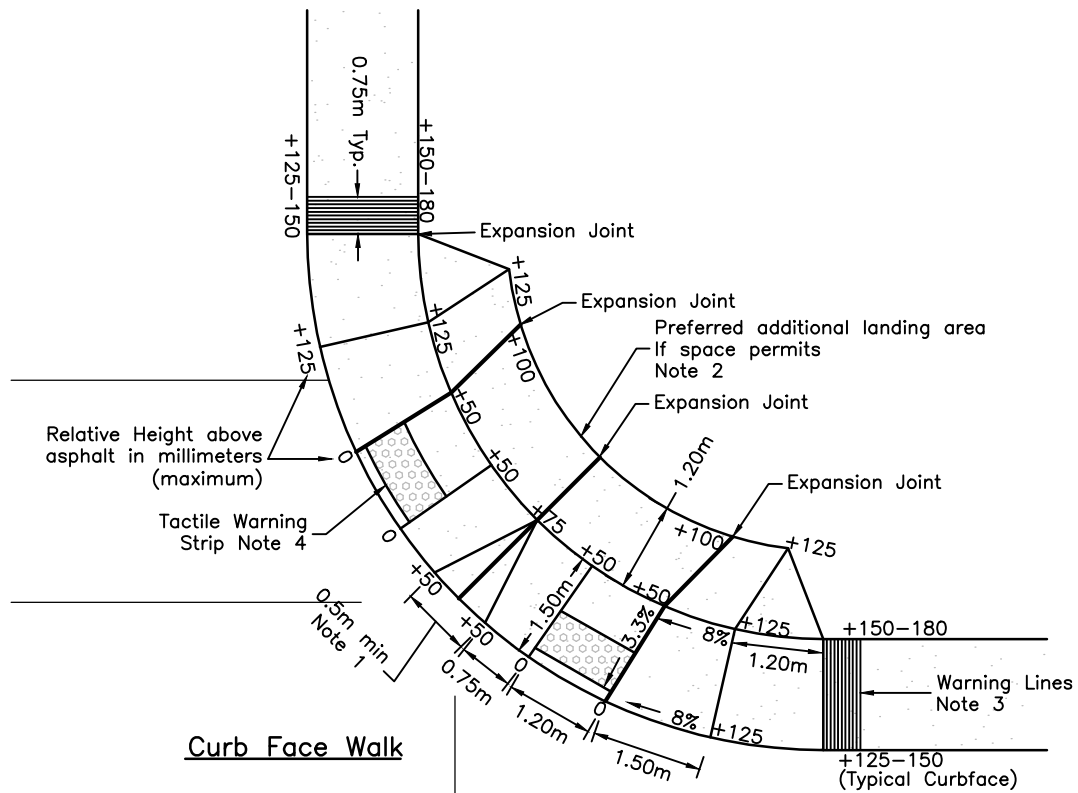
Mark Campbell, Director of Engineering, 613-546-4291 extension 3139

Other City of Kingston Staff Consulted:

Not applicable

Exhibits Attached:

Exhibit A Construction Standards



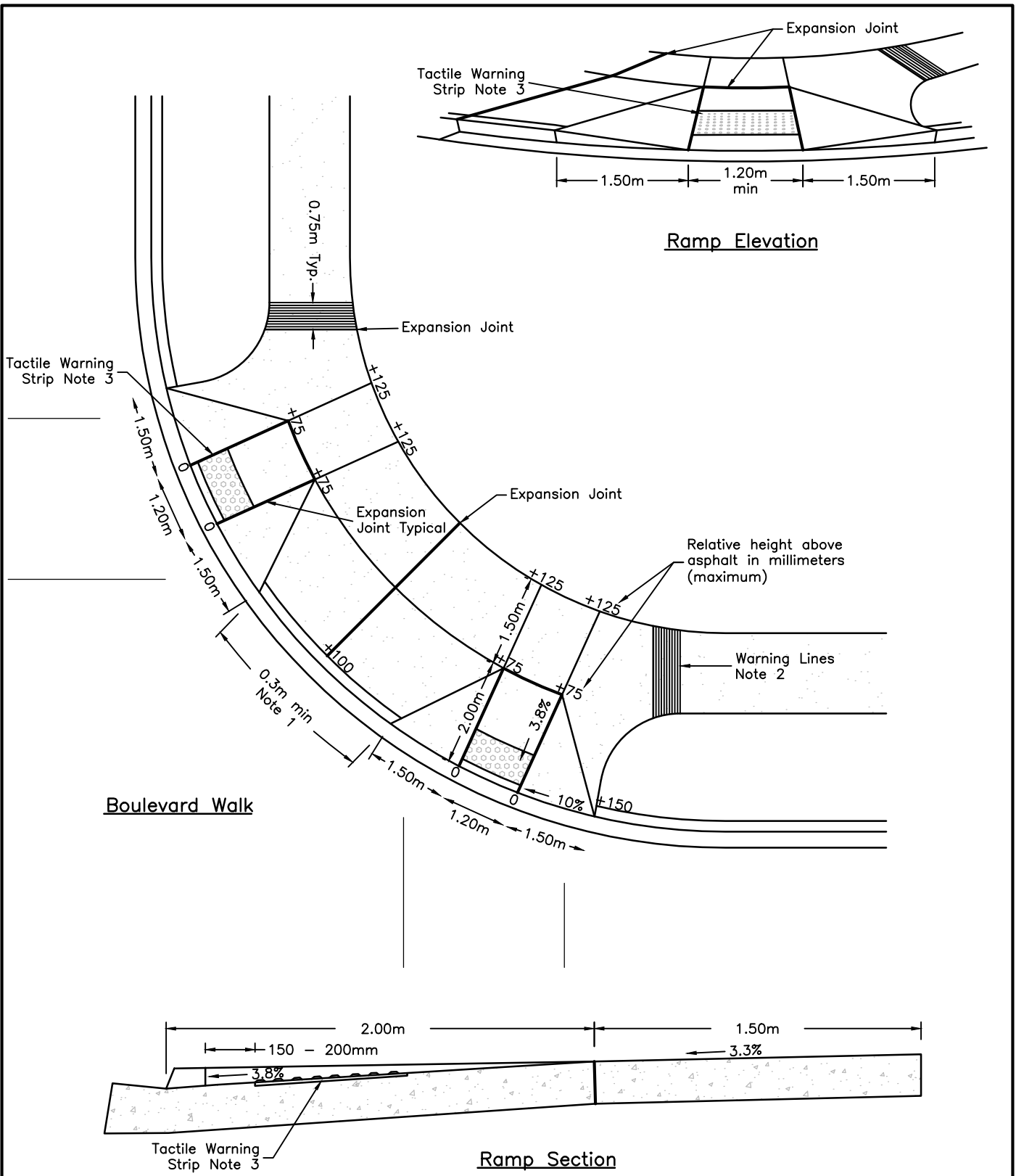
NOTES:

1. Minimum gap between ramp sections is 0.5m. If width is not available, maintain full depression between ramps.
2. Preferred additional landing area to be constructed if right-of-way width is available and normal grading standards can be followed. Addition of barrier curb at rear of landing area is permissible to manage grading.
3. Warning lines shall be 10x10mm made by a grooving tool with 50mm spacing.
4. Tactile warning strip per OPSD 310.039 or approved equivalent.

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 ENGINEERING DEPARTMENT

**Concrete Sidewalk Ramps
 at Intersections – Curb Face Walk**

Date: June 2016 Scale : NTS Dwg No: SR-01



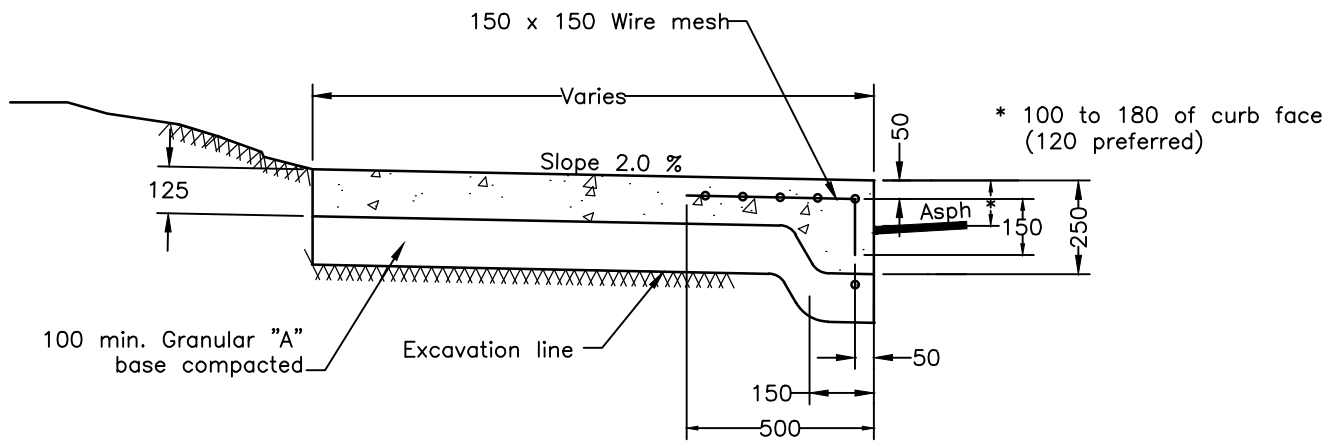
NOTES:

1. Minimum gap between ramp sections is 0.3m. If width is not available, decrease curb height to maintain 10% maximum slope on flared portion of ramp.
2. Warning lines shall be 10x10mm made by a grooving tool with 50mm spacing.
3. Tactile warning strip per OPSD 310.039 or approved equivalent.
4. No pavement marking for crosswalk where no vehicle stop required.

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ENGINEERING DEPARTMENT

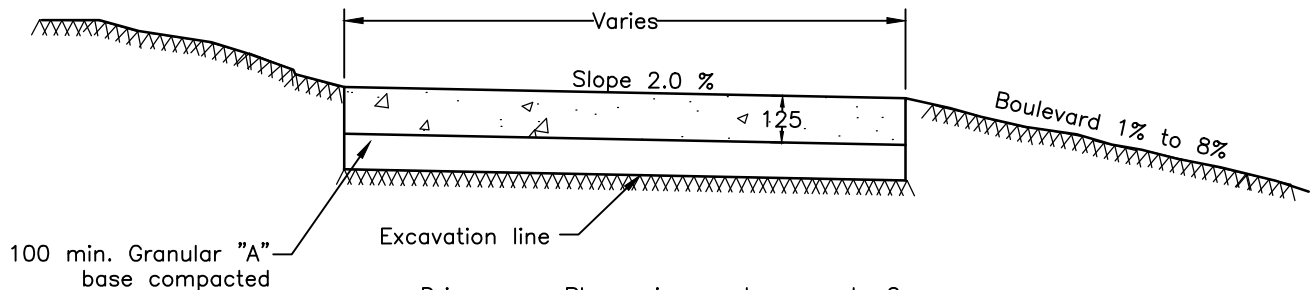
**Concrete Sidewalk Ramps
at Intersections – Boulevard Walk**

Date: June 2016 Scale : NTS Dwg No: SR-02



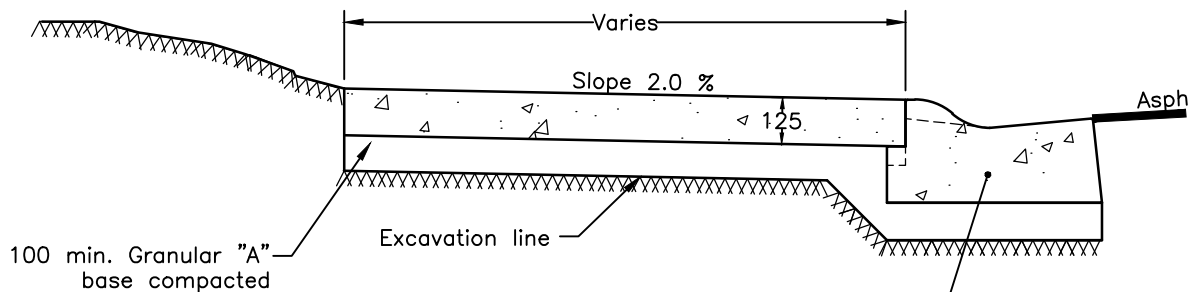
See DE-02 for driveway depression

TYPE "A" WALK



Driveways : Place wire mesh per note 2

TYPE "B" WALK



Driveways : Place wire mesh per note 2

Curb and Gutter per drawings.
(Drop curb at entrances per City of Kingston standard DE-01)

TYPE "C" WALK

ALL DIMENSIONS ARE IN MILLIMETRES

NOTES:

1. Concrete strength per contract specifications
2. Place 150x150 wire mesh in commercial and industrial entrances.

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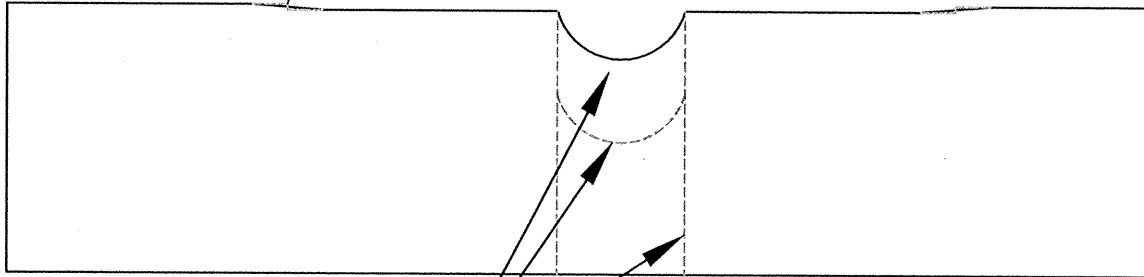
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**SECTION OF STANDARD
ONE-COURSE CONCRETE WALK**

Revised Feb. 2, 2016

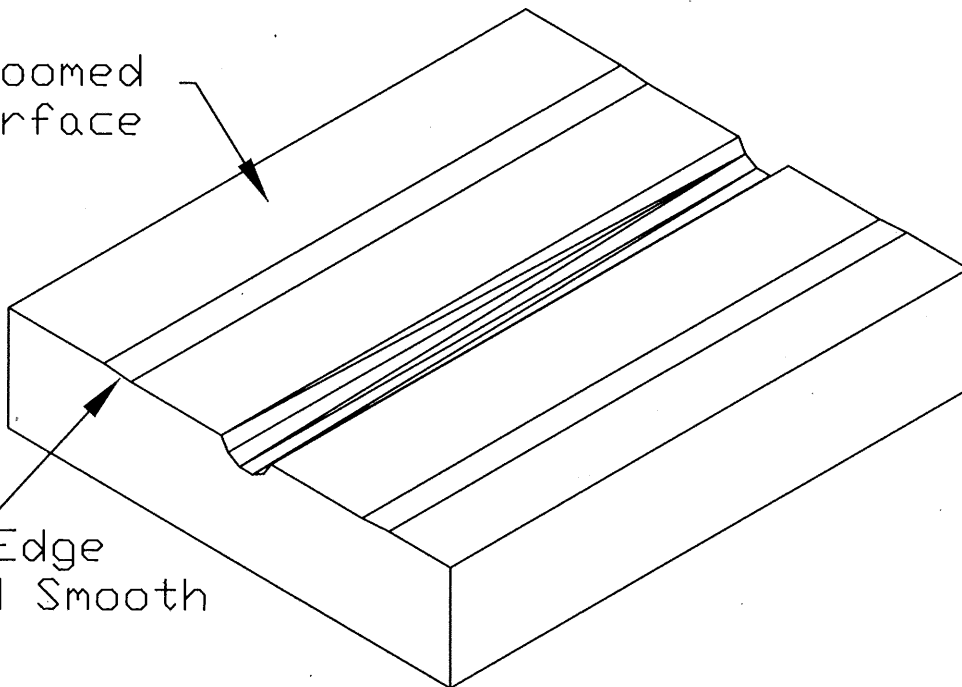
D-183-M

Tooled Edge Maximum
1mm Depth
Per OPSS 351



For All Joints -
Dummy, Contraction
and Expansion

Broomed
Surface

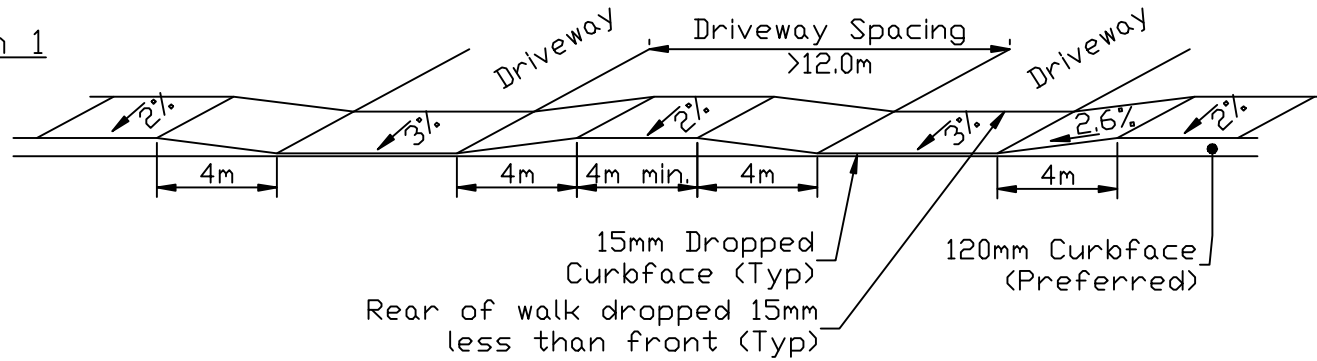


Tooled Edge
Broomed Smooth

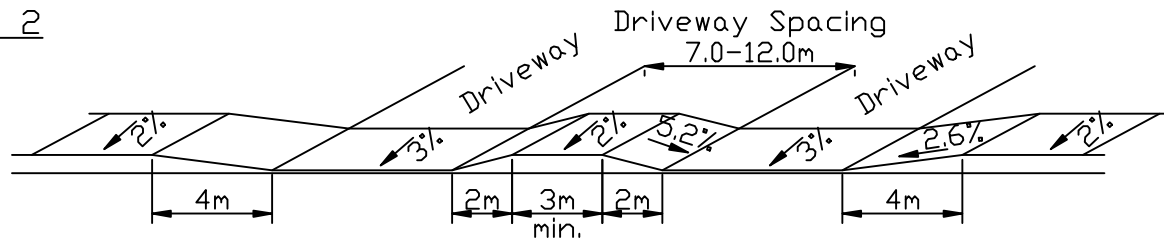
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Infrastructure Management

**Concrete Sidewalk
Joint Finishing Detail**

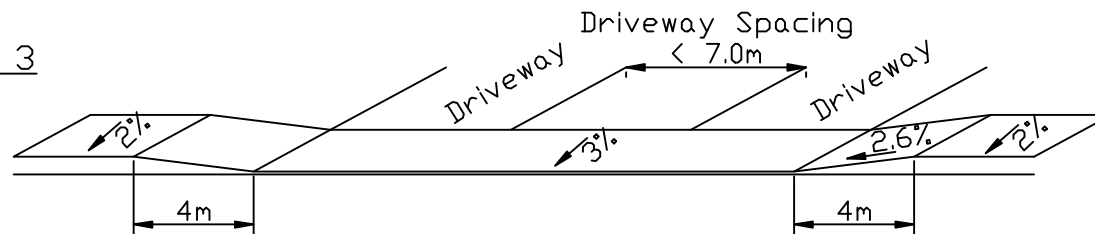
Condition 1



Condition 2



Condition 3



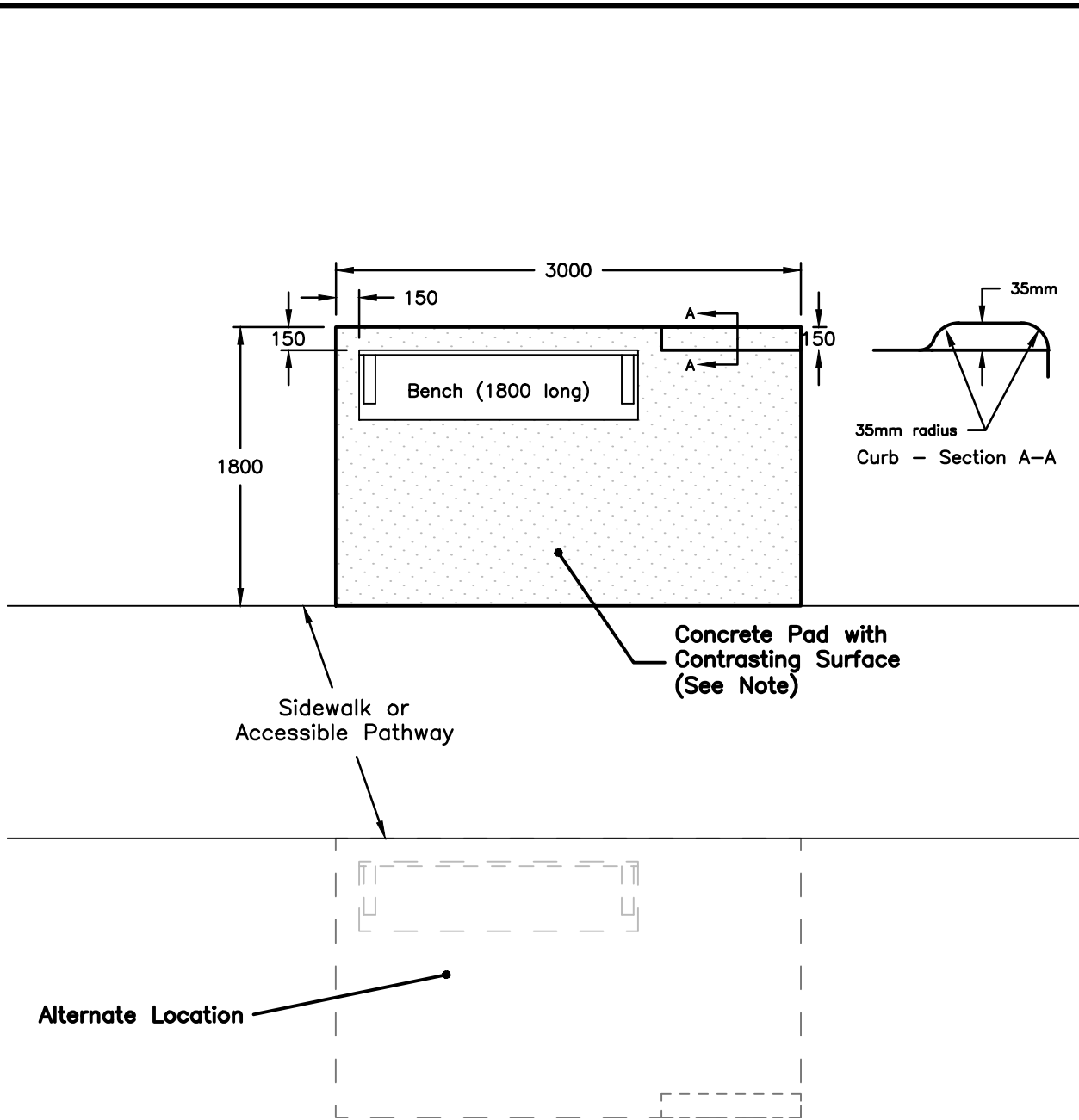
Notes

- For use with City of Kingston Standard D-183-M, Type "A" Walk
- Curbface at driveways 15mm maximum

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**Sidewalk Depression for
Curbface Sidewalk at
Driveway Entrances**



Note:
 The concrete pad shall be 125mm thick and coloured using Smoke Grey, Interstar NR-5105R (1 Bag).

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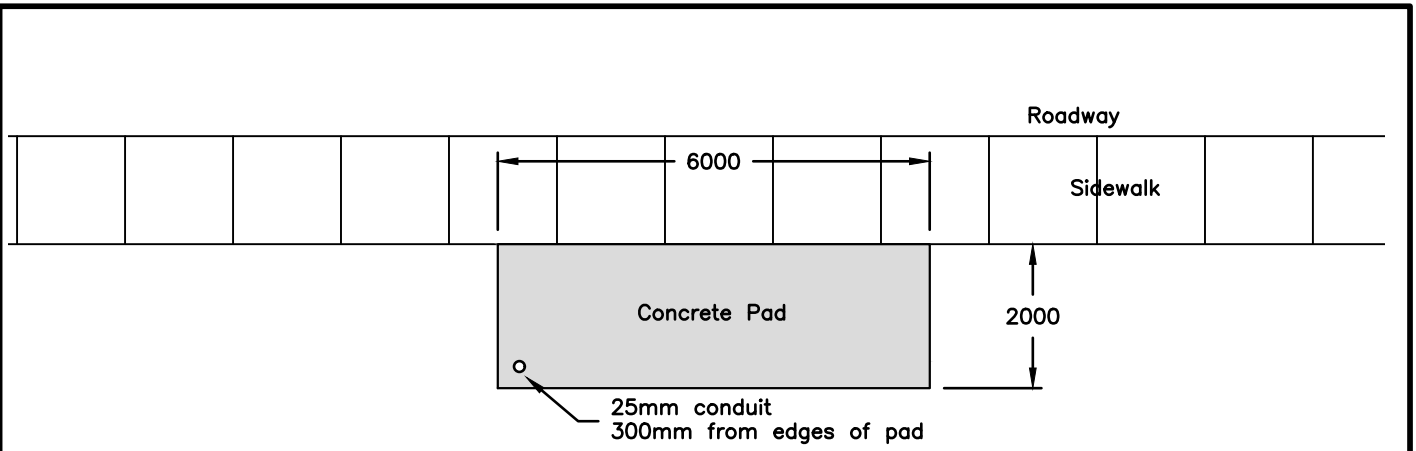
**Rest Area
 Details**

All dimensions are in millimetres.

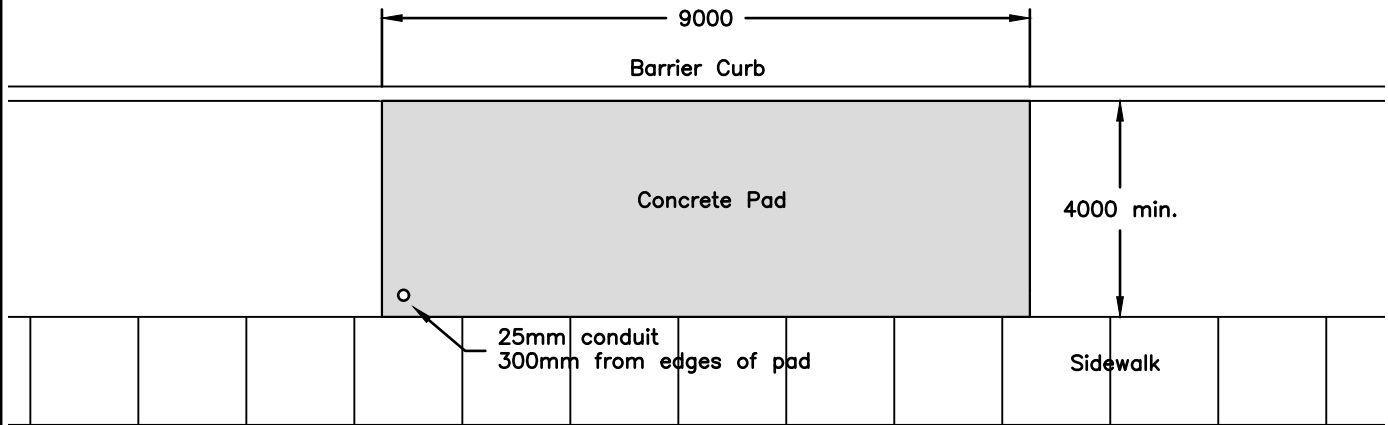
Date: 2 Feb 2016

Figure RA-1 (Rev 2)

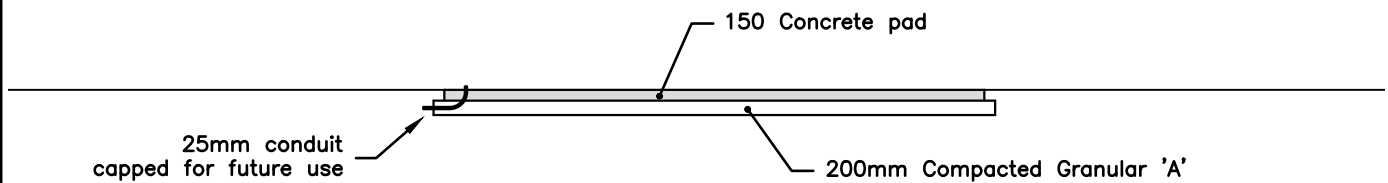
j:\Standards - City\Rest Area(Rev1).dwg



Bus Pad Details – Type 'A'
with Sidewalk Adjacent to Roadway



Bus Pad Details – Type 'B'
in Boulevard between Curb and Sidewalk



Bus Pad Details

Notes:

1. Concrete pads to be in accordance with OPSS and City of Kingston standards.
2. All pads to be sloped 2% towards the road, unless otherwise directed by the contract administrator.
3. The surface elevation of the pad must be flush to the surface elevation of adjacent grade (sidewalk or boulevard).
4. 25mm conduit to be Rigid-Sceptor.
5. All dimensions are in millimeters unless otherwise shown.

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**Bus Stop
Concrete Pad Details**

Date: Apr. 20, 2015 Dwg. BP-1